Notes:

General

1. Maximum building location envelopes shown are subject to future proposed easements and/or other underground services.

8.5

Site Cover 2. Site cover (excluding eaves, sun shading devices, patios, balconies and other unenclosed structures) does not exceed

Car Parking Spaces

3. Minimum of 2 onsite car parking spaces are provided per dwelling.

that outlined in the Site Cover Table.

Garages and Carports

4. Garage and carport openings shall be as per Garage and Carport Openings Table.

5. Driveway crossovers are located in accordance with this Plan of Development.

6. Driveways do not include a reversing bay, manoeuvring area or visitor parking space(s) (other than tandem spaces) in the front setback.

Driveways

- 7. Each dwelling includes a bin storage area that:
- is not visible from public areas or screened from public areas;
- has a minimum area of 1.0m x 2.0m
- if located within the garage, the area must be ventilated; - if located within the front setback, must be stored in a small enclosure.

Casual Surveillance

- 8. Dwellings generally must address primary frontages with a minimum of a window and either a front door or pedestrian entrance. Dwellings on corner lots may have a front door addressing the secondary frontage but must still address the primary frontage with a window on each level.
- 9. The building must have a window with an area of at least 1m² or multiple habitable room windows having a combined area of at least 2.5m2 that face the street.

Corner lots

10. For corner lots, the Primary Frontage shall be considered the narrow frontage addressing a road.

11. Built to boundary walls shall be as per Built to Boundary Walls

Built to Boundary Walls

Residential Uses Table. Setbacks

	511	E COVER					
uilding Height		Lot Size					
diffulling Height	300m² or less	301 - 400m²	401 - 500m²	501m² +	M		
.5m or less	75%	70%	60%	60%			

CITE COVER

Primary or Secondary Frontage Covered car space opening(s) per street frontage				
Greater then 18m	Not specified			
12.5m to 18m	6m wide maximum			
Less than 12.5m	Single storey dwelling: a. maximum 50% of the front frontage width (being the frontage vehicle access is from); and b. recessed: i. at least 1.0m behind the main building line; or ii. at least 1.0m behind a front portico and no more than 2.0m in front of the main building line.			
	Two storey dwelling: a. 6.0m wide maximum; and b. recessed 1.0m behind the front wall or balcony of upper level Editor's note - Front wall is to have a minimum length of 40% of the adjoining frontage.			

Lot frontage width	Mandatory / Optional	Length and Height of Built to Boundary Wall		
More than 7.5m to 12.5m	Mandatory - one side	Max Length: 60% of the length of the boundary		
		Max Height: 4.5m		
More than 12.5m to 18m	•	Max Length: the lesser of 15m or 60% of the length of the boundary		
	ii. where the built to boundary wall adjoins a lot with a frontage less than 18m.	Max Height: 4.5m		
	Not permitted - Otherwise			

BUILT TO BOUNDARY WALLS RESIDENTIAL USES

Not permitted* Greater than 18m Note - Max Length includes the length of walls of any other buildings on the same boundary. e.g. detached garage, carport or shed.

Note - For the maximum height of domestic outbuildings refer to the examples that achieve aspects of the performance outcomes for building

*Note - Built to boundary walls are not permitted, however, reduced side and rear boundary clearances may be permitted as prescribed (e.g.

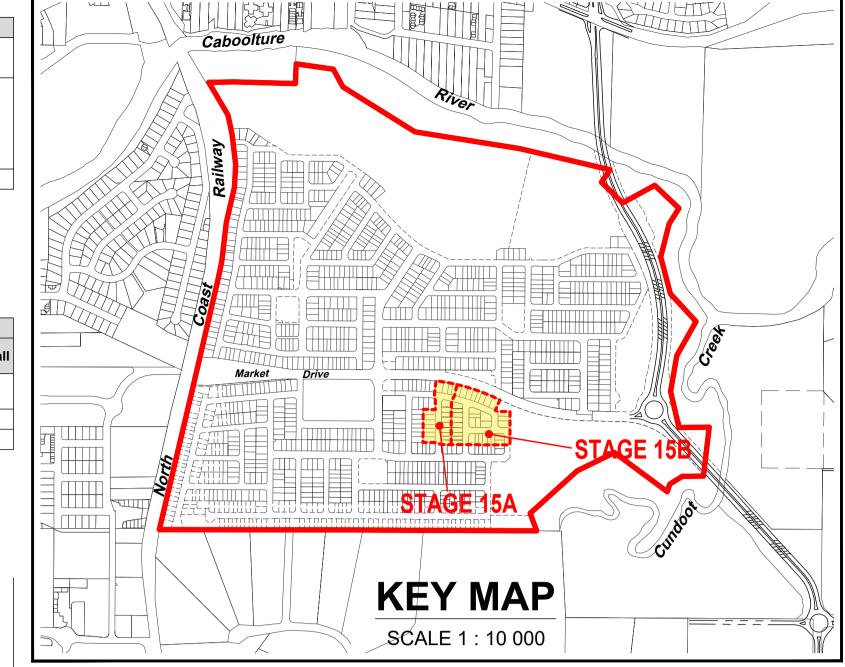
PLAN OF DEVELOPMENT SETBACKS

	Frontage				Side	Rear		
Height of Wall	Primary			Secondary to street			Non-built to boundary wall	To OMP and wall
	To Wall	To OMP*	To covered car parking space	To Wall	To OMP*	To covered car parking space	To Wall	To OMP* and Wall
Less than 4.5m	Min. 3m	Min. 2m	Min. 5.4m	Min. 2m	Min. 1m	Min. 5.4m	Min. 1m**	As per QDC
4.5m to 8.5m	Min. 3m	Min. 2m	N/A	Min. 2m	Min. 1m	N/A	#Min. 1m**	As per QDC

* OMP includes architectural features such as piers, pylons, eaves, etc.

** Eaves may encroach to a maximum of 450mm.

One side must be setback a min. 1.5m to the wall. Note - This is a qualifiable standard that is an alternative provision to QDC, part MP1.1, A1 (a), (b) and (c), A2 (a), (b) and (d) and part MP1.2,



RIVERBANK

Proposed Subdivision Stages 15A & 15B

PLAN REF: 7025 - 205L 30 OCTOBER 2019 CLIENT: DRAWN BY:

Legend

CHECKED BY:

Subject Site

Stage Boundary

Mandatory Built to Boundary Wall Nominal Built to Boundary Wall

Preferred Driveway Location Private Open Space

Indicative On-Street Parking

2.5m Shared Pedestrian/Cycle Path

2.0m Shared Pedestrian/Cycle Path

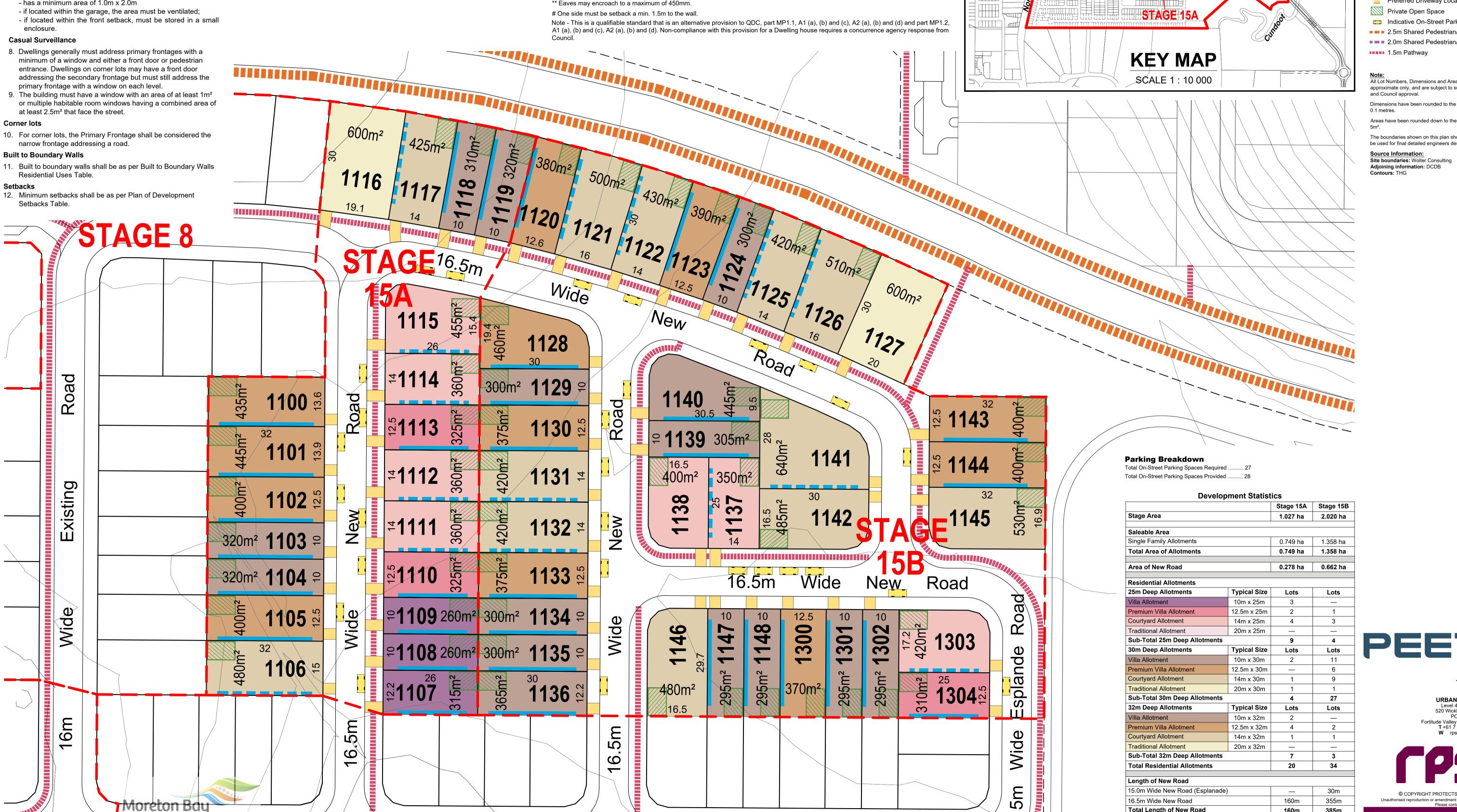
All Lot Numbers, Dimensions and Areas are

Dimensions have been rounded to the nearest

approximate only, and are subject to survey

Areas have been rounded down to the nearest

The boundaries shown on this plan should not be used for final detailed engineers design.



Parking Breakdown Total On-Street Parking Spaces Required 27 Total On-Street Parking Spaces Provided 28

Total Length of New Road

Development Statistics

Stage 15A Stage 15B

Stage Area	1.027 ha	2.020 ha	
Saleable Area			
Single Family Allotments	0.749 ha	1.358 ha	
Total Area of Allotments		0.749 ha	1.358 ha
Area of New Road		0.278 ha	0.662 ha
Residential Allotments			
25m Deep Allotments	Lots	Lots	
Villa Allotment	10m x 25m	3	_
Premium Villa Allotment	emium Villa Allotment 12.5m x 25m		
Courtyard Allotment	ourtyard Allotment 14m x 25m		
Traditional Allotment			
Sub-Total 25m Deep Allotments	9	4	
30m Deep Allotments	Typical Size	Lots	Lots
Villa Allotment	10m x 30m	2	11
Premium Villa Allotment	12.5m x 30m	_	6
ourtyard Allotment 14m x 30m		1	9
Traditional Allotment	1	1	
Sub-Total 30m Deep Allotments	4	27	
32m Deep Allotments Typical Size		Lots	Lots
Villa Allotment	10m x 32m	2	_
Premium Villa Allotment	12.5m x 32m	4	2
Courtyard Allotment	14m x 32m	1	1
Traditional Allotment	20m x 32m	_	_
Sub-Total 32m Deep Allotments	7	3	
Total Residential Allotments		20	34
Length of New Road			
15.0m Wide New Road (Esplanac	_	30m	
16.5m Wide New Road	160m	355m	

160m